

BOAT

International



CROWN JEWEL

Princess Yachts rules the semi-custom kingdom with its majestic M Class superyachts. Now there's a new member of the family, the beautifully appointed 35M

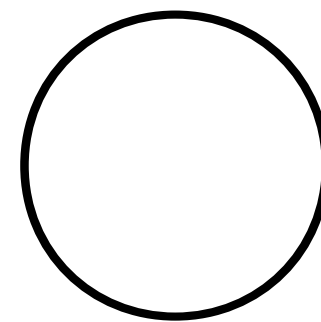
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Photographer - Quin Bisset/Princess Yachts





Above: natural light abounds in Antheya III's spacious main saloon, thanks to floor-to-ceiling windows. Left: the floating stairwell between main and lower decks is the biggest "wow" factor aboard, while the master suite (right) and main deck dining area (below) show off the panelling and textures of which Princess Yachts' director of creative design is particularly proud



ne way to gauge how much an owner loves his newly launched superyacht is to see how they behave during a boat show. When the first Princess Yachts 35M, *Antheya III*, made her debut at the Cannes show this year, her owner rarely left the boat. He sat in the shade of the aft deck, pleased with the positive reaction from visitors but entirely uninterested in becoming

part of the action. "I'm not here!" he said with a smile, waving away any attempted introductions. He preferred to blend into the background, but had no intention of relinquishing his place on board. This is an owner who plans to put his yacht to good use, so why waste any time?

This satisfaction has not come as a complete surprise as he's owned four Princesses over 15 years, his boats growing in size along with the British builder's offerings. Now he's stepped up from a Princess 32M of the same name, *Antheya* being a combination of his children's names. It's rather apt: three generations of the family will enjoy *Antheya III* and they all influenced her design.

It's only natural that such a devoted Princess owner would take delivery of hull No.1 of the 35M, a yacht dubbed an "evolution" of the builder's 32 and 40 metre models. Notable changes are seen inside and out - as well as below the waterline with a newly optimised hull. Bernard Olesinski Limited worked in collaboration with Princess Yachts' in-house designers and engineers to develop the hull form, exterior styling and layout of the 35M, while the interior is styled by the Princess Design Studio. Because it was hull No.1, the owner was able to influence the yacht's specifications and design, shaping much of what would go into the latest family member in the M Class range.

The Plymouth-based builder took the semi-custom market by storm with the launch of its M Class series in 2011, with the flagship 40M, *Imperial Princess*, picking up a World Superyacht Award in 2013. The range now runs to three models, with a new 30M replacing the old 32M at the lower end, but the 35M is no neglected middle child. If anything, she shines brighter than her siblings. The 35M's impressive foredeck lounge, for one, makes that of the 40M look tiny by comparison. A forward-facing bench beneath the pilothouse is fronted by two L-shaped settees and tables, all of which can be shaded with an awning, and forward yet of this are two big sunpads, enjoying the best view in the house. As Justin Olesinski, managing director of the Bernard Olesinski design studio, explains: "The 40M really pushed the boundaries and, in essence, we took what we learnt from the 40M and put it into the 35M."

Exterior design samples both its big and little sisters, hitting a styling note that is immediately Princess. Despite the fact that the 35M is a raised pilothouse yacht and the 40M a tri-deck, striking similarities are in the exterior styling. The window design on the forward deck master is the same on both, with cat's eye-shaped glazing cut into the hull just below another window in the superstructure, letting ample light into the master and giving a defining geometry to the exterior lines.

"We wanted the 35M to fit in the range between the 32M and 40M - like a BMW, you have to keep it looking similar even though it's

different," Olesinski says. "The forward end [of the 35M] is similar to the 40M, while the aft deck is more similar to the 32M. We've picked some of the best features of the yachts either side to create a winner."

The 35M has a sporty, aggressive look about her, an aim of the designers. "We always try to reduce the height from the outside, while inside increasing the headroom," says Olesinski. "The 35M is lower than similar yachts in her size range, which not only makes her look faster and sleeker, but it also increases seakeeping abilities because the centre of gravity is lower."

It's an important attribute for a yacht designed with an eye on performance. It took Bernard Olesinski more than a year to develop *Antheya III*'s optimised hull, during which time the design studio investigated more than 100 hull forms with its in-house CFD and proprietary Hull & Hydro software.

The progressive V-hull is a hybrid design that's efficient at both semi-planing and displacement speeds. Not only is the 35M faster than her big sister, but drop down to 12 to 14 knots in standard cruise, and she's 20 per cent more efficient. Aided by gyro stabilisers, she's extremely sure on her feet as well. The added efficiency will be a benefit to *Antheya III*'s owners, who plan to log many miles in their preferred cruising grounds of the Mediterranean with the kids and grandkids in tow.

"This boat has had an enormous influence from the client because he's got three generations of his family on board," says Sarah Verey, director of creative design at Princess. "We've had to explore practicality as well as the beauty of the boat." As such, there is no open shelving to show off knick-knacks. Instead, much discreet storage is built-in, which you might miss unless you knew where to look. The glassware is cleverly concealed in columns in the dining area, as Verey points out: "When you open it up it's beautifully lit and shows the cut glass, which is nice as well as practical."

All the berths lift to hold linen for the cabin; the crew accommodations have under-floor storage you can climb into for dry stores; there is a huge bosun's locker at the bow; and the garage stows a 5.65 metre tender and a wet bike. Maximising space is a trick that Princess has honed over half a century building smaller motor yachts. Andrew

Lawrence, head of design at Princess, says: "We're always chasing every last millimetre of space and we took that philosophy forward into the bigger boats, maximising the volume. With the 35M it's all about interior volume and bringing light in; that's what sells boats these days."

The 35M really lives up to this mantra, introducing huge picture-box portlights on the lower deck that are larger than those on her predecessors. There are floor-to-ceiling windows on the main deck and sliding glass doors on either side that open on to fold-down balconies, making the main saloon a true indoor-outdoor living space.

The biggest wow factor inside, though, is the floating stairwell. Stairways and lower-deck foyers on yachts of this size can often be narrow, dark spaces, and the goal was to open this up. A full-height glass bulkhead frames the solid, leather-wrapped baluster and walnut flooring treads. It's a striking piece that's also an engineering marvel.

Another unique feature on *Antheya III*'s main deck is the flock of birds that wing their way across the glass backdrop in the dining saloon and the leather headboards in the master cabin. The 3D feature, known ▶

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Above: the main deck aft offers not only alfresco dining but, unusually, aft-facing seating for a view over the transom. Right: the raised pilothouse looks out on the stunning foredeck lounge, while the hydraulic swimming platform, (below) brings the water into play whenever you want it



The sundeck of the Princess 35M is a huge and versatile space. Antheya III's owner chose the custom layout, with a dining table under the hardtop, and forward and aft-facing seating

as “Flight”, was stumbled upon accidentally when Princess’s design studio was experimenting with a new kind of panelling, but it will be offered as standard on future builds. It’s a moment of engaging textural design, one of many found on board. “It is about textures,” says Verey. “That’s something I work on really hard, rather than having a pattern in it, it has to have a texture.”

Verey’s influence can be seen in the geometric pattern throughout the timber in the wall panelling, which is complemented by the Rubelli hand-dragged gold wallpaper, cut into the same veneer as the timber. Interesting materials abound, such as the liquid metal finishes on the saloon console tops.

Despite the fact that *Antheya III* is draped in Louis Vuitton blankets and dotted with Fendi pillows during her public debut at the Cannes show – a nod to Princess Yachts’ ownership by LVMH – this is clearly much more than a showpiece yacht. She will be lived in, and elements such as her bamboo carpeting, which meets the owner’s brief for hypoallergenic materials, speaks to this. Bamboo carpet is also a practical choice as it can help dampen vibration and noise levels, and is better value and easier to clean than many other carpets.

The owner also had his say in the third lower-deck suite, which features a family lounge layout. Multiple layouts are available for the 35M, the standard being a five-cabin interior with a main deck master and four guest cabins on the lower deck. On *Antheya III*, the owner merged the two aft twin cabins to make a single, large lounge. The starboard side has a berth, while the port side has a sofa and TV.

It’s an intimate family space where the generations can gather for games or films. Come bedtime, a partition slides shut, dividing the room. The lounge sofa converts into a bed and, once again, there are four guest cabins.

Crew accommodation is found forward on the lower deck, with the crew area linked to the galley so staff can move freely between the working zones without getting underfoot. That being said, the owner of *Antheya III* gets on well with his crew and, as such, the pilothouse is specifically designed with a welcoming lounge seating area and round table where he can spend time with the captain during the journey.

The owner also chose the custom layout for the flybridge, which sees a round table under the shade of a hardtop, a midline bar and seating and sunpads aft. Previous Princesses were customisable to an extent – the furnishings on the fly were often moulded in, so there was only so much you could change – but here more loose items have been used to give owners greater freedom with the layout of their top deck.

Hull numbers two and three of the Princess 35M are already in build and number four is being moulded. A fifth build slot is available for spring 2017. François Zuretti was chosen by the owners of the second hull to design a classic interior, marking the first time the French designer has worked with Princess.

While this first 35M will be entirely private, she’s built to meet charter code requirements, strictly with an eye to resale, for that inevitable day when her owner decides to step up yet again – to a Princess 40M, we’d be willing to wager. ■

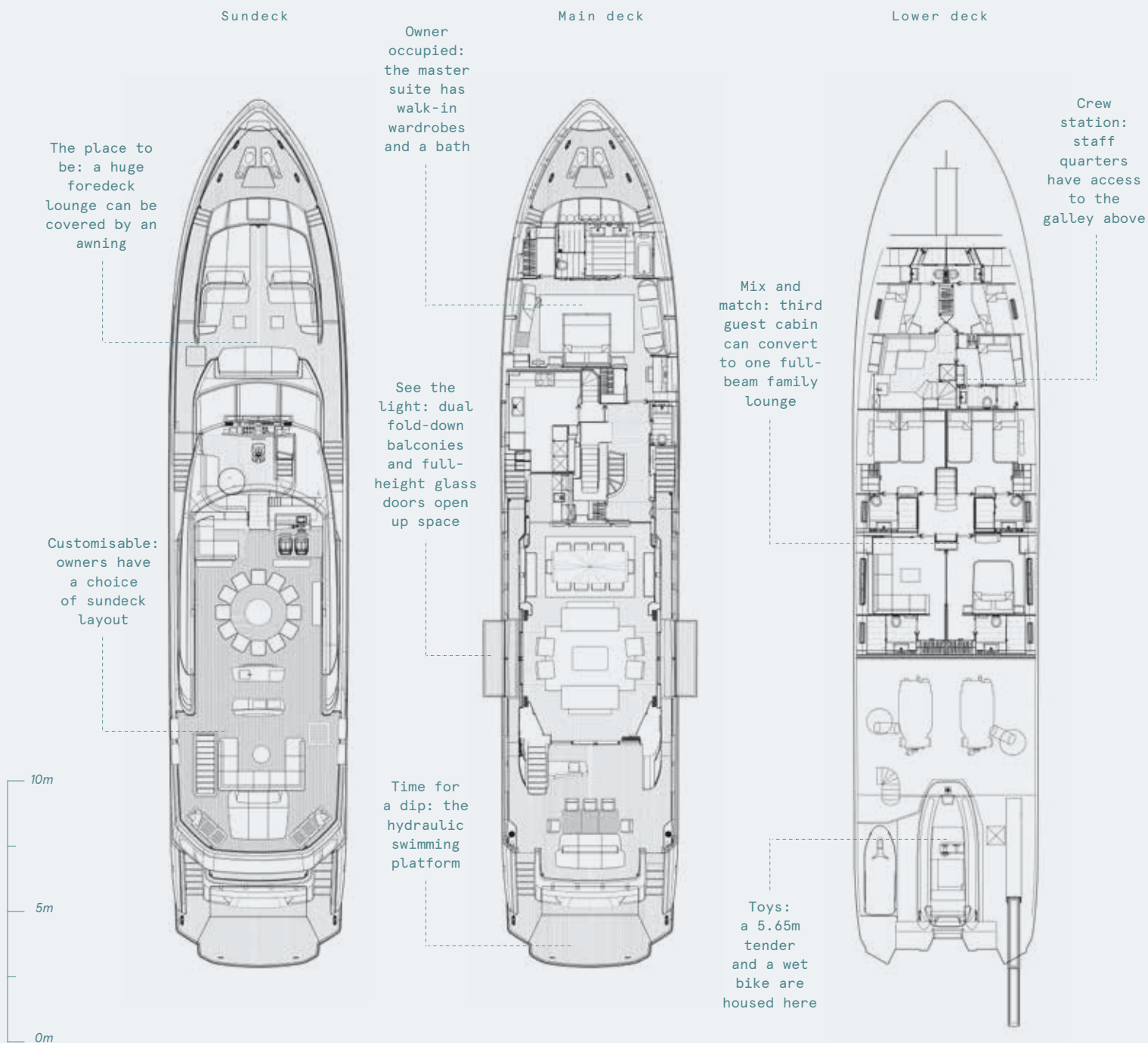
See more on the iPad edition



SPECS

ANTHEYA III/PRINCESS 35M

PRINCESS YACHTS



LOA 35.15m
Beam 7.3m
Draught 2.11m
Gross tonnage
248GT
Engines
2 x MTU 16V
2000 M94

Speed (max/cruise)
26/17 knots
Range at 10 knots
1,750nm
Generators
2 x Onan QD,
65kW
Fuel capacity

18,000 litres
Freshwater capacity
3,250 litres
Tender
1 x 5.65m Williams
DieselJet 565
Owners/guests 8/10
Crew 5
Construction

Composite
Classification
RINA Commercial,
MCA LY3 Short Range
Naval architecture
Bernard Olesinski
**Exterior and
interior styling**
Bernard Olesinski;

Princess Design
Studio
For sale
From £8.28 million,
ex-VAT
Builder/year
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International/2015
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